

NOTES:

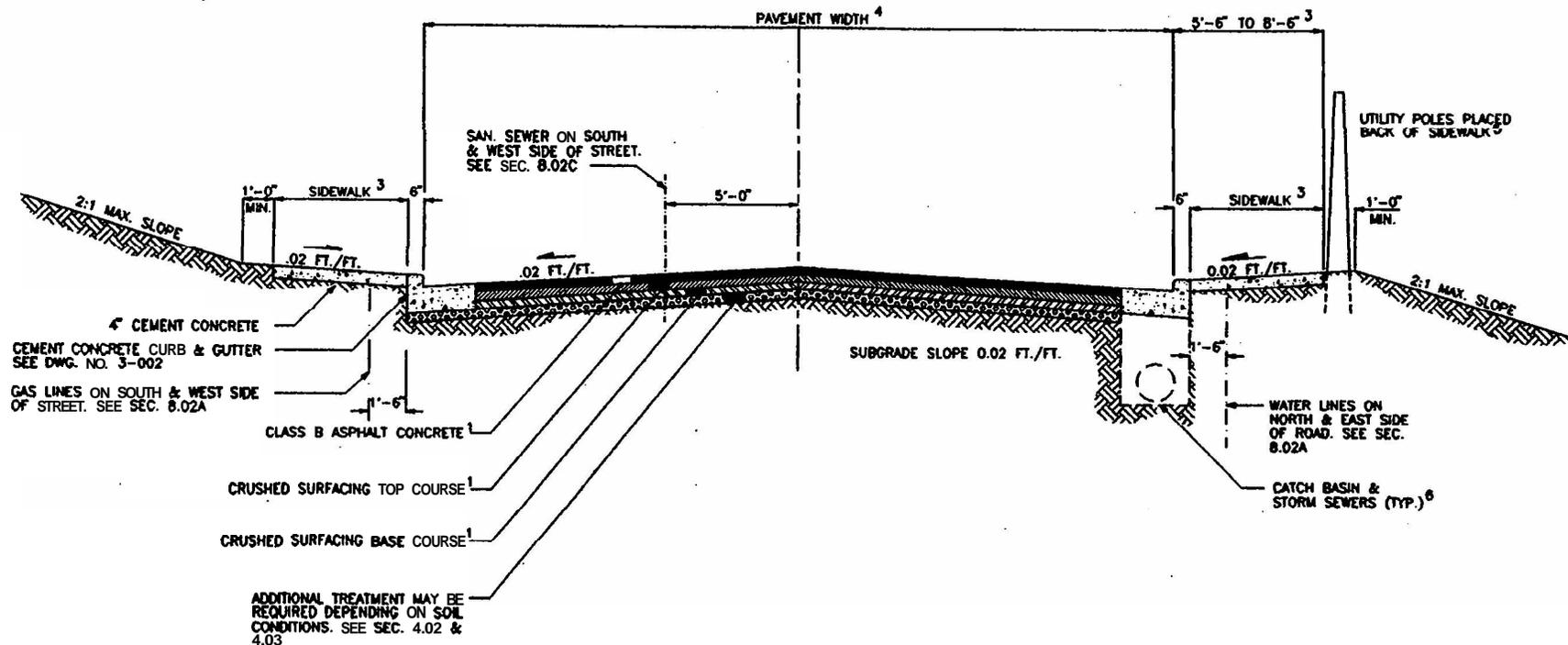
1. THIS DRAWING ILLUSTRATES A TYPICAL ASPHALT CONCRETE ROAD SECTION. ALTERNATIVE II WITH GRAVEL SHOULDERS. ACTUAL SURFACING DESIGN FOR ARTERIALS AND COMMERCIAL ACCESS STREETS SHALL BE BASED ON SOILS AND TRAFFIC ANALYSIS PER SEC. 4.03. DESIGN FOR RESIDENTIAL ACCESS STREETS SHALL BE IN ACCORDANCE WITH SECS. 4.01 AND 4.02.
2. SHOULDERS SHALL BE SURFACED AS REQUIRED BY SECS. 3.07 AND 4.01. IF PAVED, SHOULDER SLOPE SHALL MATCH CROWN SLOPE OR 0.02 FT./FT.
3. GRADES:
 MINIMUM 0.5%
 MAXIMUM SEE SECS. 2.02, 2.03, 2.04, AND 2.11.
4. FOR WIDTHS OF PAVEMENT, SHOULDER, AND RIGHT-OF-WAY. SEE SECS. 2.02, 2.03, 2.04.
5. FOR CLEARANCE OF UTILITY POLES SEE SEC. 8.02G AND DWG. NO. 5-001.
6. SEE SEC. 3.08 FOR SEPARATED WALKWAY IF REQUIRED.
7. DITCH SECTIONS AND/OR LOCATIONS MAY VARY TO MEET REQUIREMENTS OF THE SURFACE WATER DESIGN MANUAL. FOR RURAL NEIGHBORHOOD COLLECTORS SEE DWG. NO. 2-024 FOR TURNPIKE SHOULDER ALTERNATIVE.
8. SEE SEC. 5.02 FOR SIDE SLOPE REQUIREMENTS.



KING COUNTY PUBLIC WORKS
KING COUNTY, WASHINGTON

SHOULDER TYPE ROADWAY

DWG. NO. 1-001



NOTES:

1. THIS DRAWING ILLUSTRATES A TYPICAL ASPHALT CONCRETE ROAD SECTION, **ALTERNATIVE II**. ACTUAL SURFACING DESIGN FOR ARTERIALS AND **COMMERCIAL** ACCESS STREETS **SHALL** BE BASED ON SOILS **AND TRAFFIC ANALYSIS** PER SEC. 4.03. DESIGN FOR RESIDENTIAL ACCESS **SHALL** BE IN ACCORDANCE WITH SECS. 4.01 AND 4.02.
2. GRADES:
 MINIMUM **0.5%**
 MAXIMUM SEE SECS. 2.02, 2.03, 2.04, AND 2.11.
3. SIDEWALKS **SHALL** BE 8 FT. **WIDE** IN BUSINESS DISTRICTS AND 6.5 FT. **WIDE** ON ARTERIALS IF NEXT TO TRAFFIC LANE, 5 FT. **WIDE** IF NEXT TO PARKING OR **BIKE** LANE. OR BEHIND PUNTING STRIP SEE SECTION 3.02B.
4. FOR WIDTHS OF **PAVEMENT**, SHOULDER, AND RIGHT-OF-WAY, SEE SECS. 2.02, 2.03, AND 2.04.
5. FOR CLEARANCE OF **UTILITY POLES** SEE SEC. 8.02G AND DWG. NO. 5-001.
6. SEE **CHAPTER 7** FOR CATCH BASIN AND STORM SEWER **LOCATIONS**.
7. SEE SEC. 5.02 FOR **SIDE** SLOPE REQUIREMENTS.

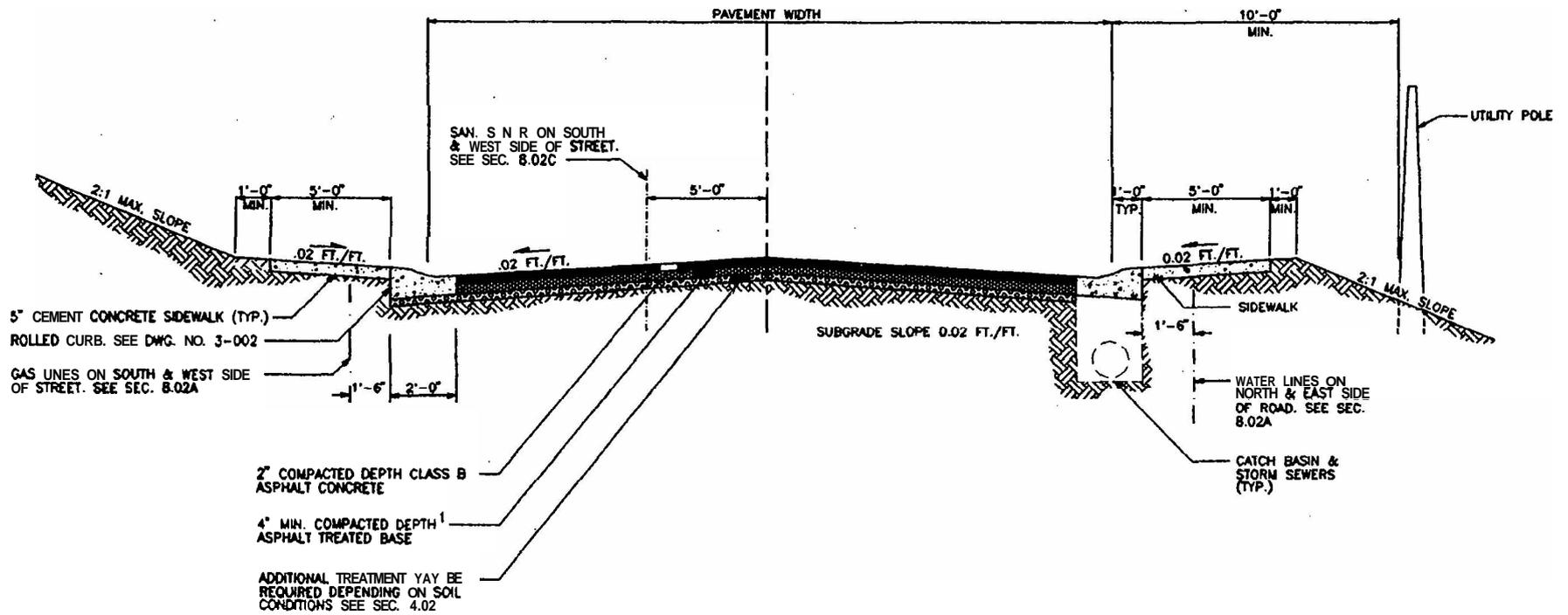
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KING COUNTY PUBLIC WORKS
 KING COUNTY, WASHINGTON

VERTICAL CURB TYPE ROADWAY

DWG. No. 1-002



NOTES:

1. THIS DRAWING ILLUSTRATES A TYPICAL ASPHALT CONCRETE ROAD SECTION. ALTERNATIVE I. FOR OTHER ALTERNATIVES AND POSSIBLE **REQUIREMENTS** FOR FRACTURED AGGREGATE OR INCREASED THICKNESS OF SURFACING **MATERIALS**, SEE SECS. 4.01 AND 4.02.
2. GRADES:

MINIMUM	0.5%
MAXIMUM	SEE SECS. 2.03 AND 2.11.
3. SEE CHAPTER 7 FOR CATCH **BASIN** AND STORM SEWER LOCATIONS. SEE DWG. **NO.S 2-019, 2-020, AND 2-021** FOR GRATE DETAILS.
4. FOR WIDTHS OF PAVEMENT AND RIGHT-OF-WAY. SEE SECS. 2.03.
5. FOR CLEARANCE OF **UTILITY POLES** SEE SEC. **8.02G** AND DWG. NO. 5-001.
6. SEE SEC. 5.02 FOR **SIDE SLOPE** REQUIREMENTS.

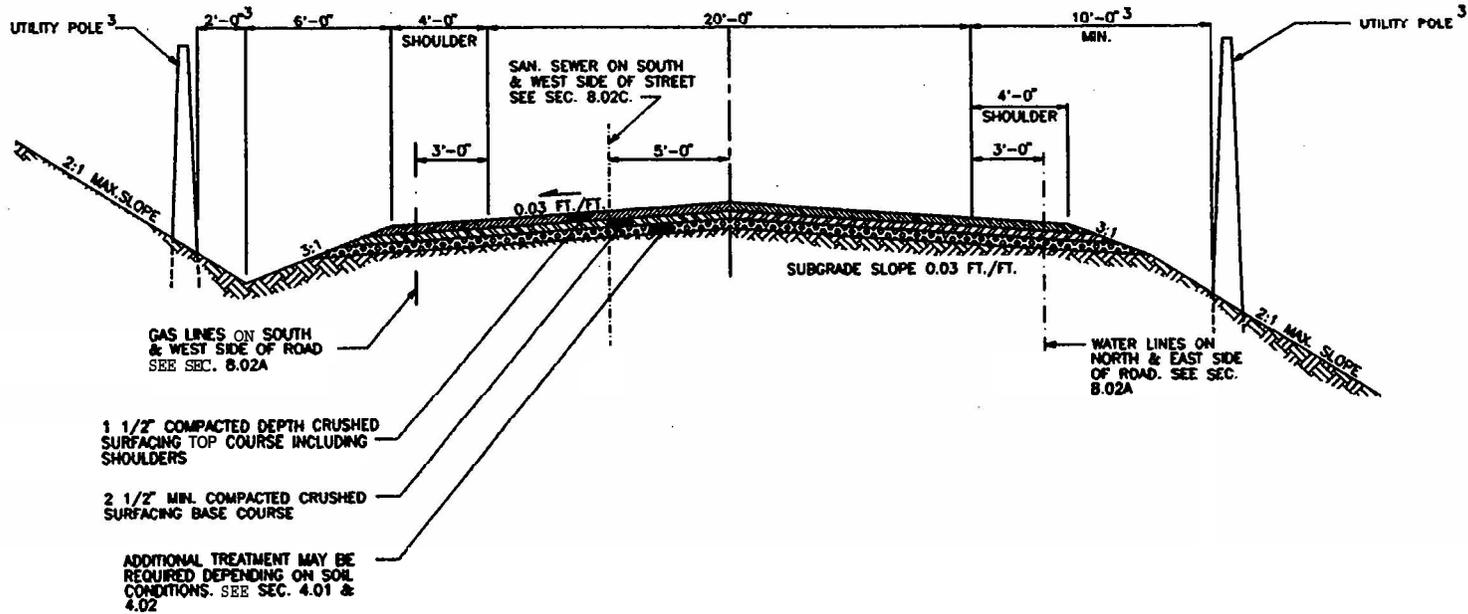
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KING COUNTY PUBLIC WORKS
KING COUNTY, WASHINGTON

ROLLED CURB TYPE ROADWAY

DWG. No. 1-003



NOTES:

1. GRAVEL SURFACING IS ACCEPTABLE ONLY ON RURAL MINOR ACCESS STREETS (RESIDENTIAL) WHICH ARE PRIVATE STREETS. SEE SECS. 2.03 & 2.17.
2. GRADES:
 MINIMUM 0.5%
 MAXIMUM SEE SECS. 2.03 & 2.11.
3. FOR CLEARANCE OF UTILITY POLES SEE SEC. 8.02G & DWG. NO. 5-001.
4. DITCH SECTIONS AND/OR LOCATIONS MAY VARY TO MEET REQUIREMENTS OF SURFACE WATER DESIGN MANUAL.
5. SEE SEC. 5.02 FOR SIDE SLOPE REQUIREMENTS.

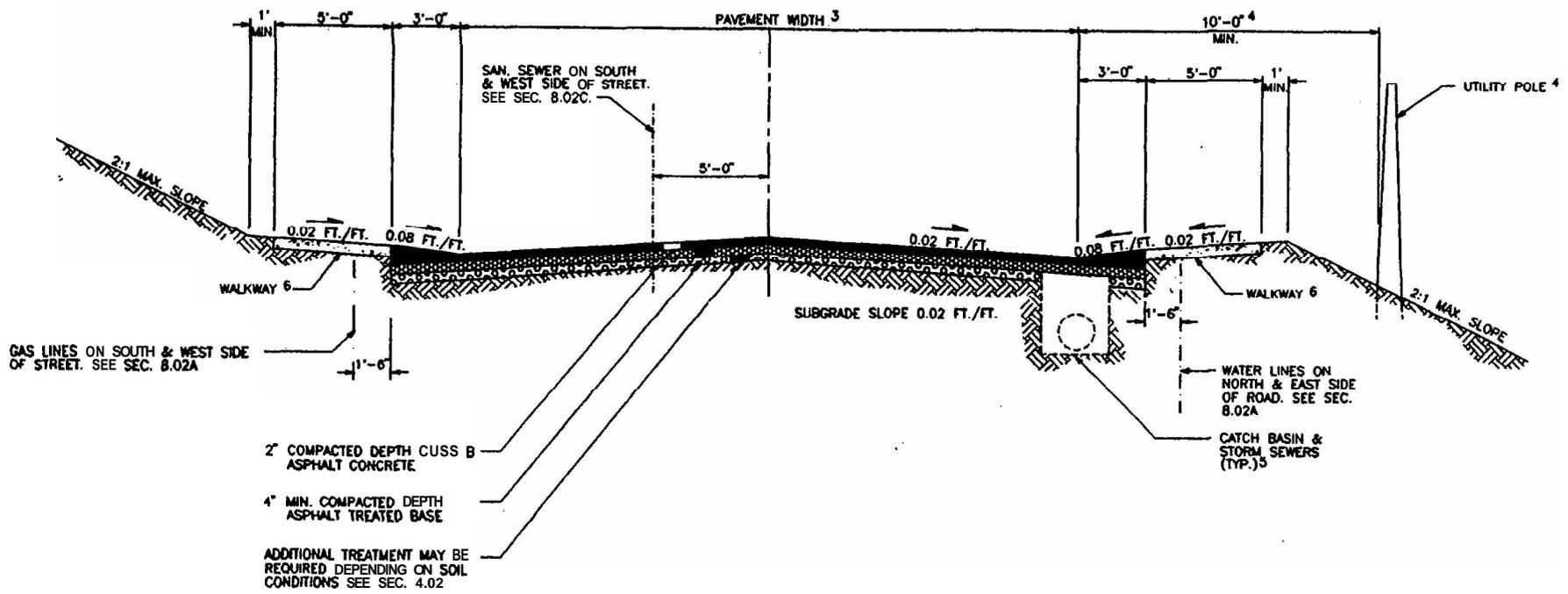
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 KING COUNTY, WASHINGTON

GRAVEL SURFACE ROADWAY

DWG. NO. 1-004



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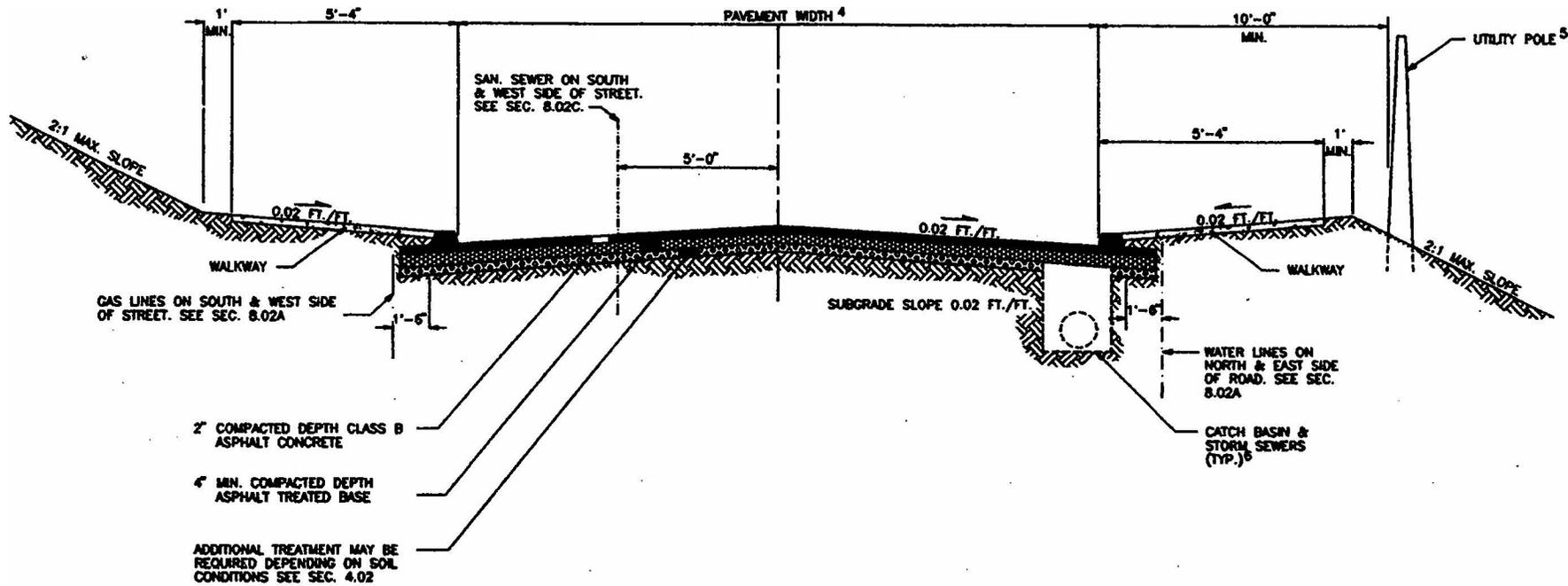
1. **THIS DRAWING** ILLUSTRATES A TYPICAL ASPHALT CONCRETE ROAD SECTION, ALTERNATM I. FOR OTHER **ALTERNATIVES AND POSSIBLE REQUIREMENTS** FOR FRACTURED AGGREGATE OR INCREASED THICKNESS OF SURFACING MATERIALS, SEE SECS. 4.01 AND 4.02.
2. GRADES:
 MINIMUM **0.5%**
 MAXIMUM SEE SECS. 2.03 AND 2.11.
3. FOR WIDTHS OF **PAVEMENT** AND RIGHT-OF-WAY. SEE SECS. 2.03.
4. FOR CLEARANCE OF **UTILITY POLES** SEE SEC. **8.02G** AND DWG. NO. 5-001.
5. SEE CHAPTER 7 FOR CATCH BASINS AND STORM SEWER LOCATIONS.
6. WALKWAY SHALL BE CEMENT CONCRETE, ALTERNATM IV, OR CRUSHED SURFACING. ALTERNATM V. AS REQUIRED BY **REVIEWING AGENCY**. SEE SEC. 4.01.
7. FOR RURAL NEIGHBORHOOD COLLECTORS, SEE DWG. NO. 2-024 FOR TURNPIKE SHOULDER ALTERNATM.
8. SEE SEC. 5.02 FOR **SIDE** SLOPE REQUIREMENTS.



KING COUNTY PUBLIC WORKS
 KING COUNTY, WASHINGTON

THICKENED EDGE ROADWAY

DWG. NO. 1-005



NOTES:

1. THIS DRAWING ILLUSTRATES A **TYPICAL ASPHALT CONCRETE ROAD SECTION, ALTERNATIVE I**. FOR OTHER **ALTERNATIVES** AND POSSIBLE REQUIREMENTS FOR **FRACTURED AGGREGATE OR INCREASED THICKNESS OF SURFACING MATERIALS**, SEE SECS. 4.01 AND 4.02.
2. FOR EXTRUDED CURB **DETAIL** SEE **DWG. NO. 3-002**.
3. GRADES:

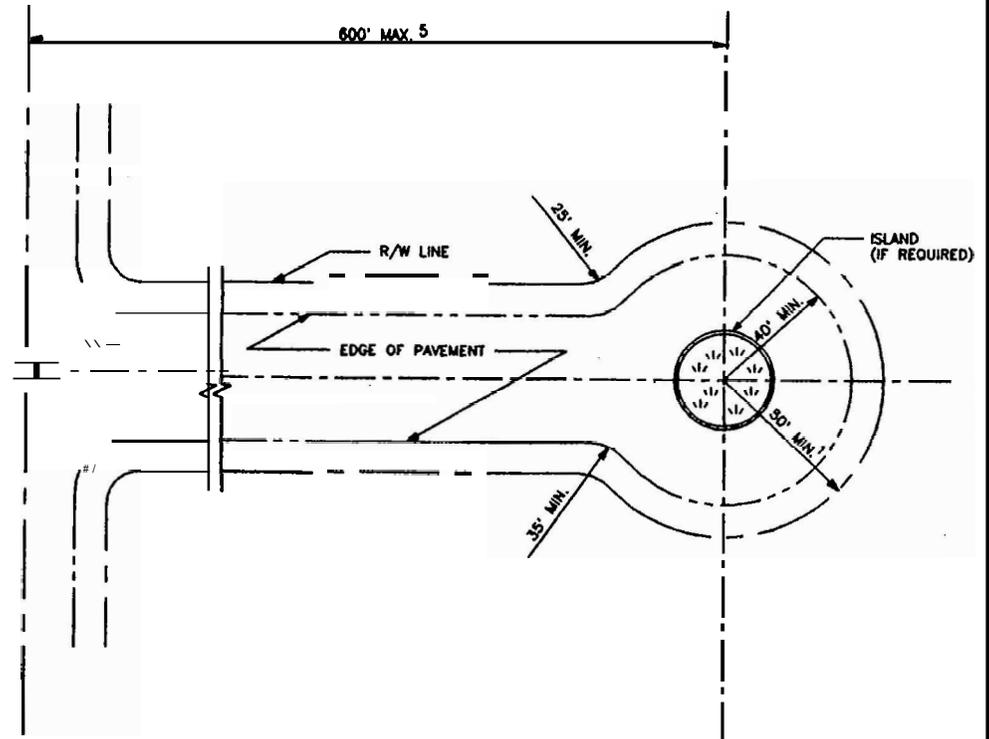
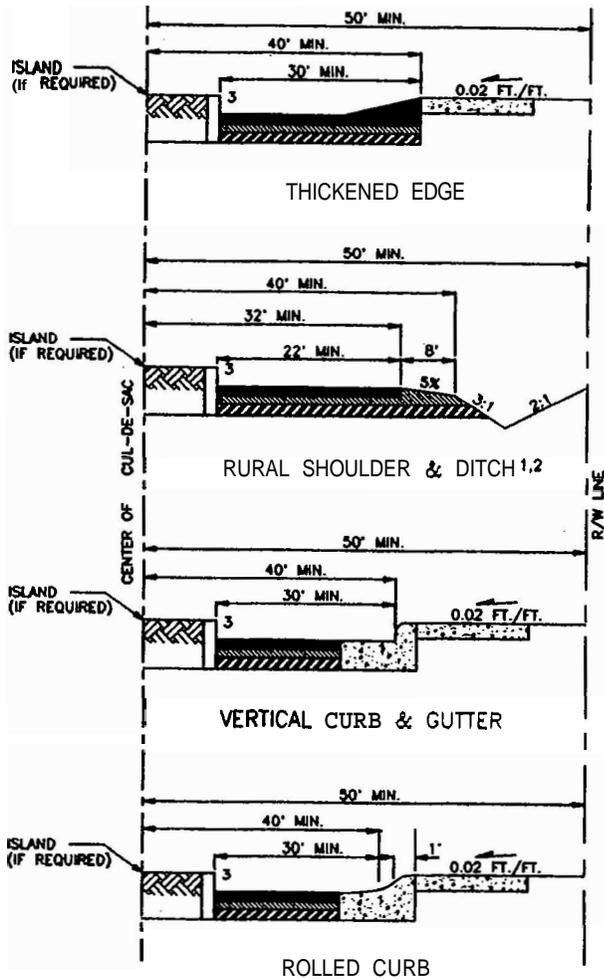
MINIMUM	0.5%
MAXIMUM	SEE SECS. 2.03 AND 2.09.
4. FOR WIDTHS OF PAVEMENT AND RIGHT-OF-WAY, SEE SECS. 2.03.
5. FOR CLEARANCE OF UTILITY POLES SEE SEC. 8.02G AND DWG. NO. 5-001.
6. SEE CHAPTER 7 FOR CATCH BASH AND STORM **DRAIN LOCATIONS**.
7. **WALKWAY SHALL BE CRUSHED SURFACING, ALTERNATIVE V OR AS REWIRED BY REVIEWING AGENCY.**
8. SEE SEC 5.02 FOR **SIDE SLOPE REQUIREMENTS**.

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EXTRUDED CURB ROADWAY

DWG. NO. 1-006



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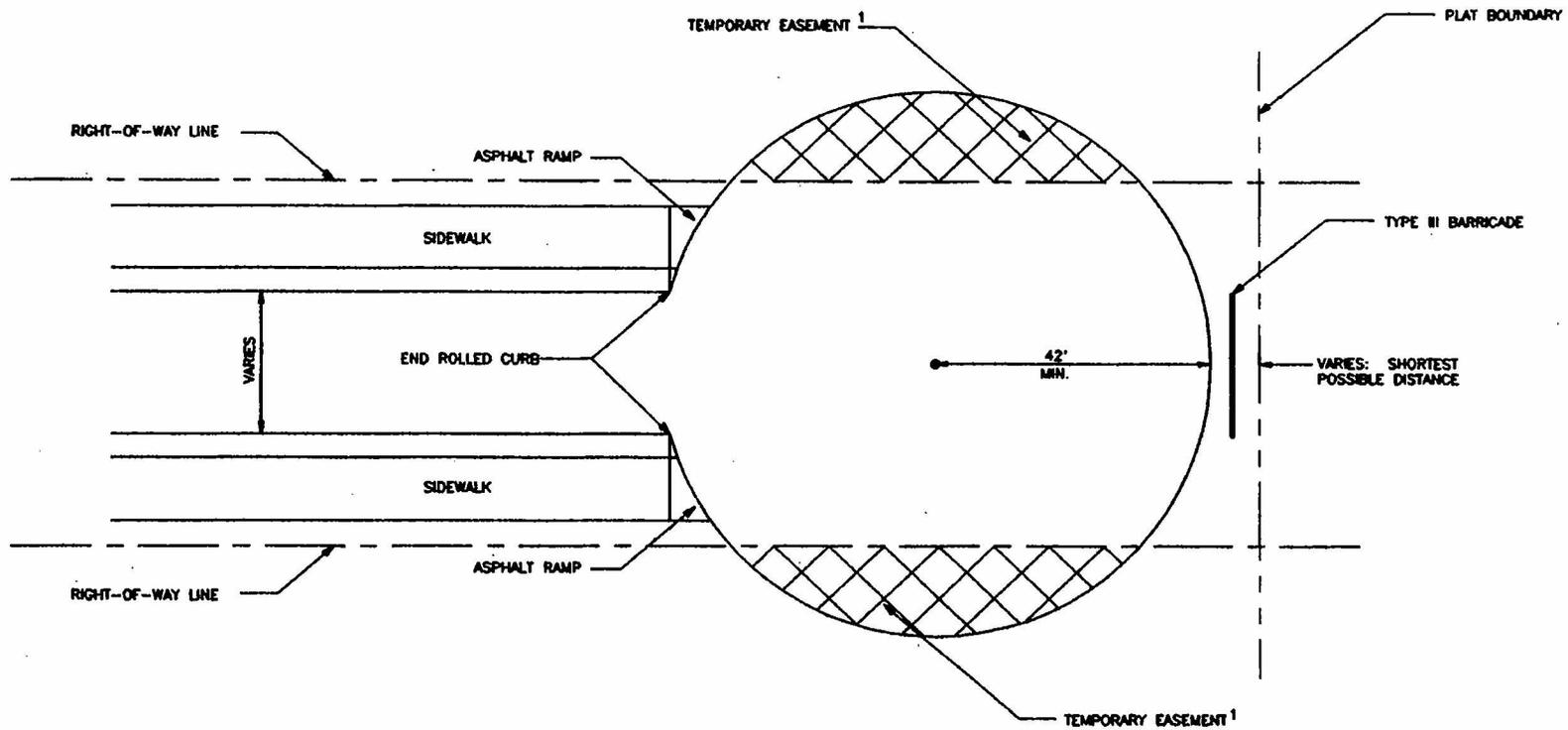
1. SEE SEC. 2.08.
2. EXTRUDED CURB IS ALSO ACCEPTABLE FOR OUTER EDGE AS ALTERNATIVE TO SHOULDER AND DITCH. SEE DWG. NO. 1-006.
3. ISLAND AT CENTER OF BULB SHALL HAVE VERTICAL CURB. SEE DWG. NO. 3-002.
4. ISLAND IS MANDATORY WHEN RADIUS OF PAVED AREA EXCEEDS 40'.
5. SEE SEC 2.08 FOR CUL-DE-SAC LENGTH EXCEPTION.
6. SEE SECS. 2.03, 2.08, AND 2.09 FOR RIGHT-OF-WAY REDUCTION REQUIREMENTS



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CUL-DE-SACS

DWG. NO. 1-007



NOTES:

1. SEE SEC. 2.08.
2. BARRICADE REQUIRED AT END OF BULB.
SEE SEC. 5.07.

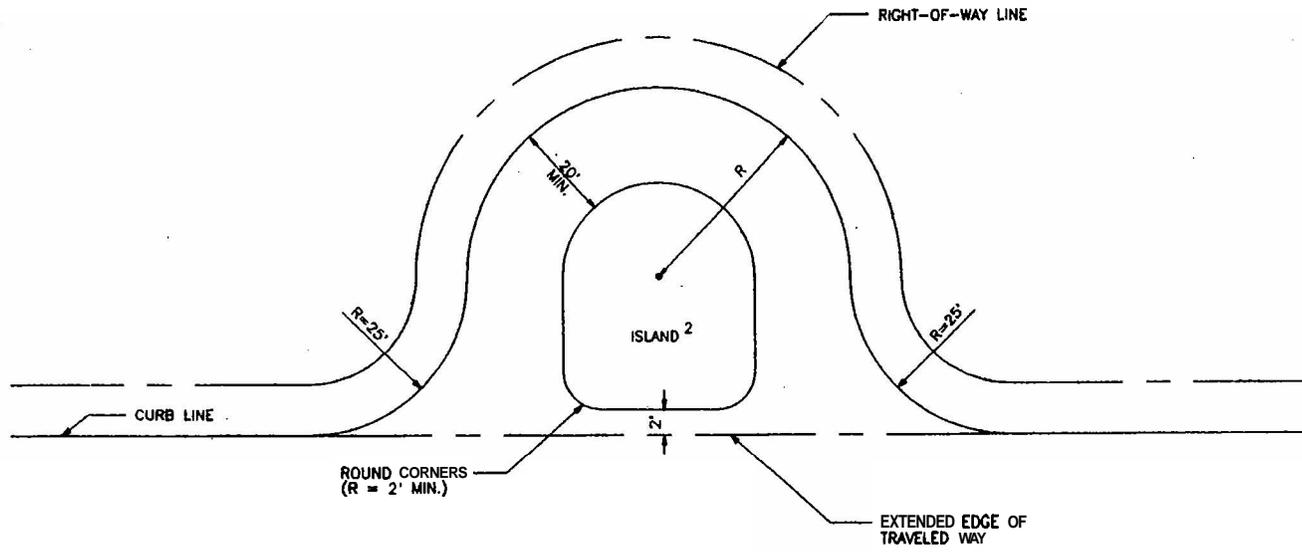
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KING COUNTY PUBLIC WORKS
KING COUNTY, WASHINGTON

TEMPORARY CUL-DE-SAC

DWG. 1-008
NO.



NOTES:

1. SEE SEC. 2.08F.
2. ISLAND REQUIRED ON EYEBROWS WITH R GREATER THAN 25 FEET.
3. MIN. ISLAND DIAM. SHALL BE 10 FEET.

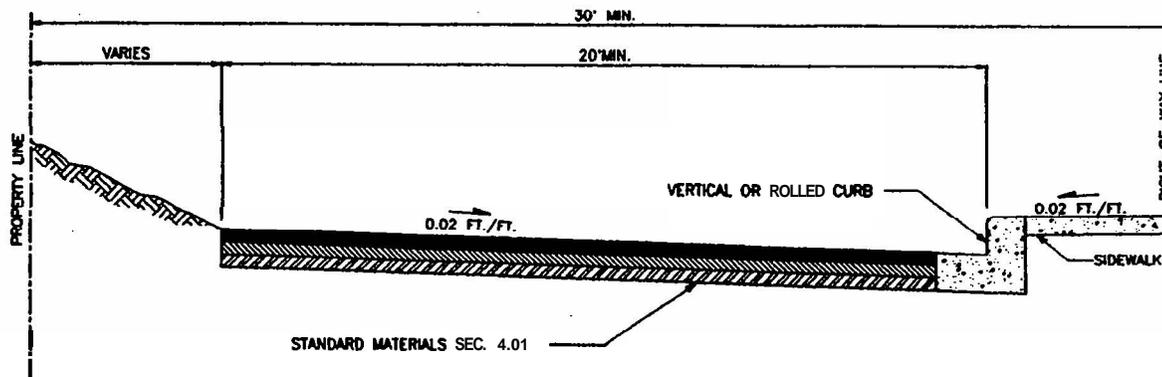
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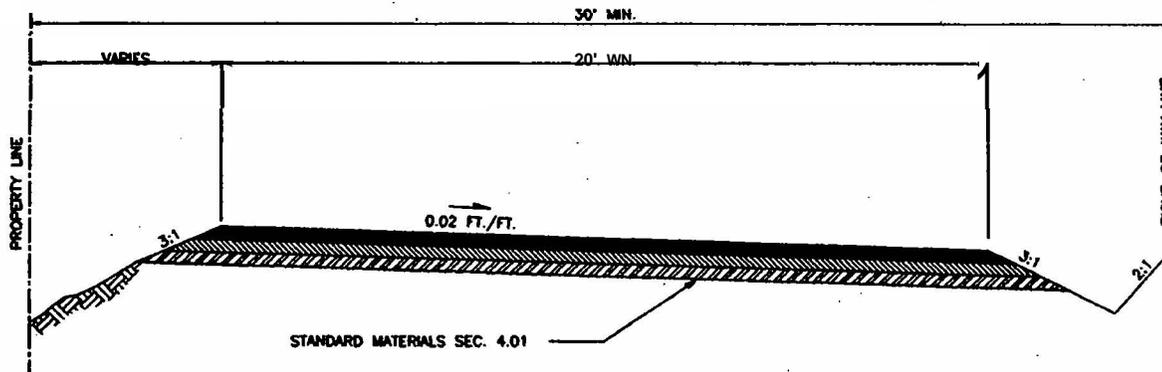
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EYEBROW

DWG. NO. 1-009



URBAN



RURAL

NOTE:

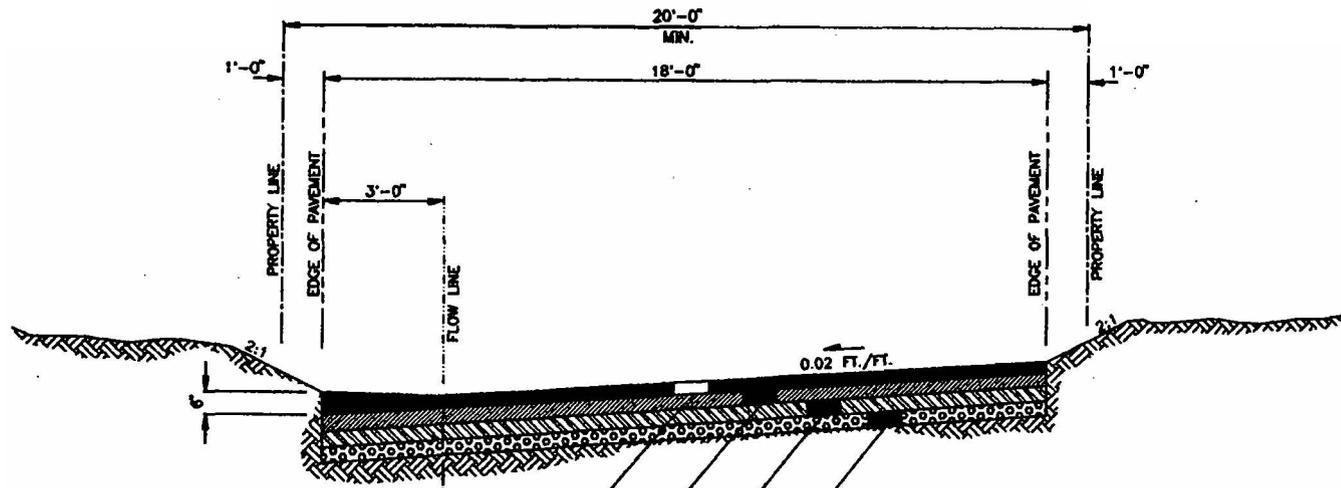
1. SEE SEC. 2.07.
2. SEE SEC. 3.02.
3. EDGE OF PAVEMENT TO BE CONSTRUCTED AS SHOWN FOR CUT OR FILL SECTION AS APPROPRIATE.



KING COUNTY PUBLIC WORKS
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HALF-STREET

DWG. 1-010
NO.



- 2" COMPACTED DEPTH CLASS B ASPHALT CONCRETE
- 1 1/2" MIN. COMPACTED DEPTH CRUSHED SURFACING TOP COURSE
- 2 1/2" MIN. COMPACTED DEPTH CRUSHED SURFACING BASE COURSE
- ADDITIONAL TREATMENT MAY BE REQUIRED DEPENDING ON SOIL CONDITIONS. SEE SEC. 4.02

NOTE:
SEE SEC. 2.09

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ALLEY PAVEMENT DETAIL

DWG. NO. 1-011