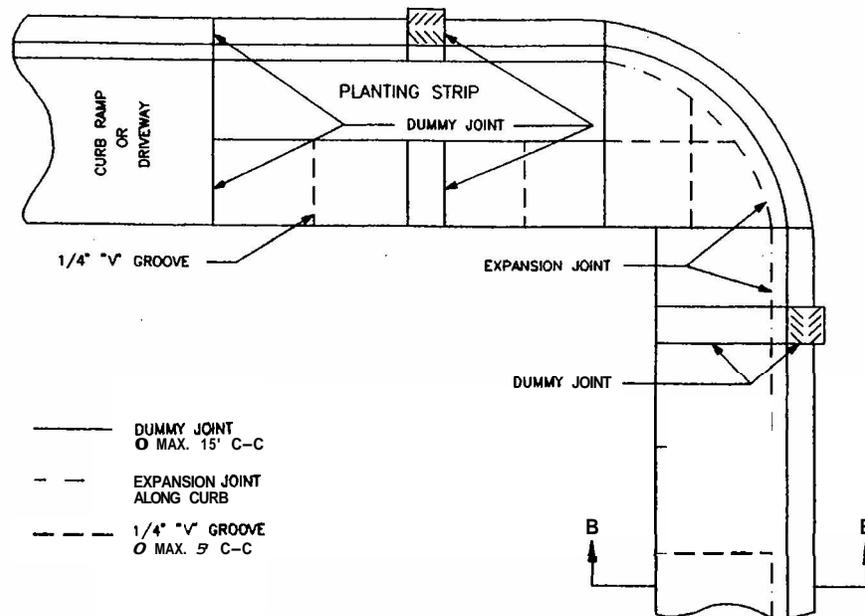
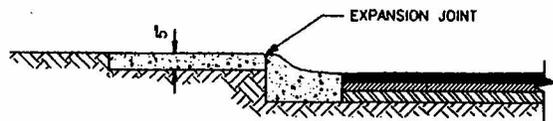


ROLLED CURB & SIDEWALK

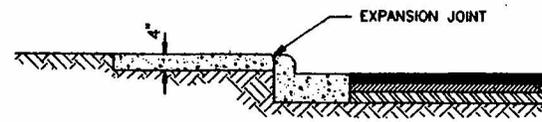


VERTICAL CURB & SIDEWALK

——— DUMMY JOINT
 O MAX. 15' C-C
 - - - EXPANSION JOINT
 ALONG CURB
 - - - 1/4" "V" GROOVE
 O MAX. 5' C-C



SECTION A-A



SECTION B-B

NOTE:

1. SEE SEC. 3.04 FOR JOINT REQUIREMENTS.
2. FOR INTEGRAL POUR CONSTRUCTION, 1/4" EDGED GROOVE MAY REPLACE EXPANSION JOINT AT INTERFACE BETWEEN CURB AND ADJACENT SIDEWALK.

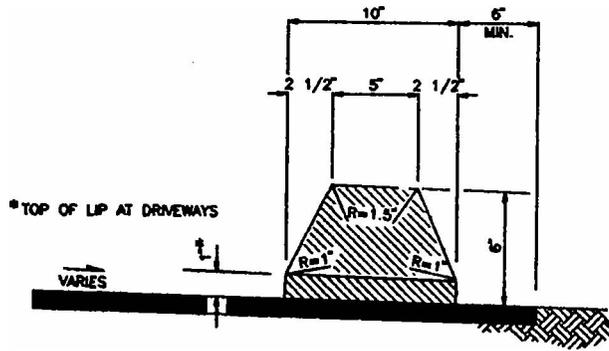
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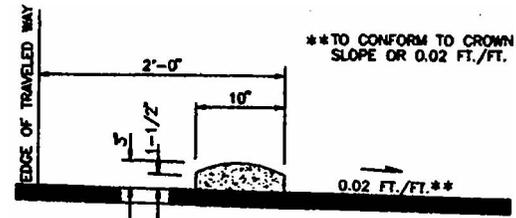
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CURB & SIDEWALK JOINTS

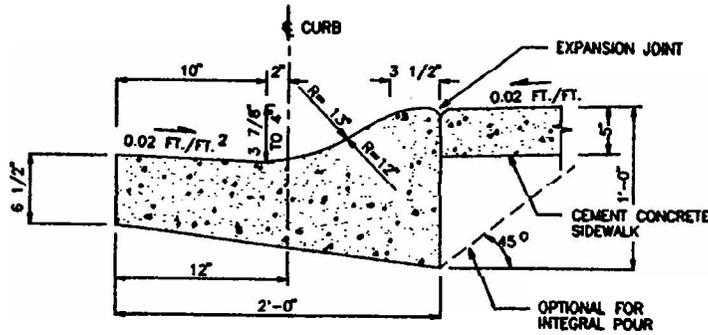
DWG. NO. 3-001



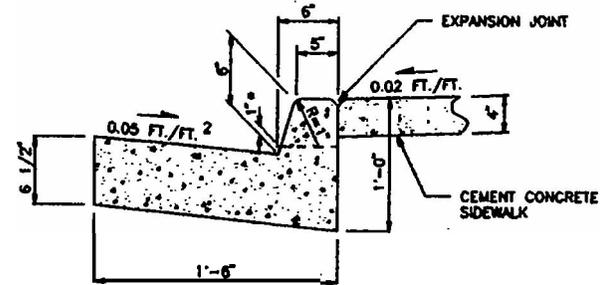
EXTRUDED ASPHALT OR CEMENT
CONCRETE CURB 3.5



MOUNTABLE CEMENT CONCRETE CURB



CEMENT CONCRETE ROLLED CURB



* NOTE: TOP OF UP AT DRIVEWAYS.

CEMENT CONCRETE CURB & GUTTER

NOTES:

1. SEE SEC. 3.04 KCRS. FOR JOINT REQUIREMENTS.
2. ROLL GURER TO MATCH POSITIVE SUPERELEVATION.
3. SEE DRAWING NO. 1-006 FOR CONFIGURATION OF FILL & WALKWAY BEHIND CURB IF REQUIRED.
4. FOR INTEGRAL POUR CONSTRUCTION, 1/4" EDGED GROOVE MAY REPLACE EXPANSION JOINT AT INTERFACE BETWEEN THE CURB AND ADJACENT SIDEWALK.
5. SEE SEC. 3.03 FOR EXTRUDED CURB ANCHORAGE.

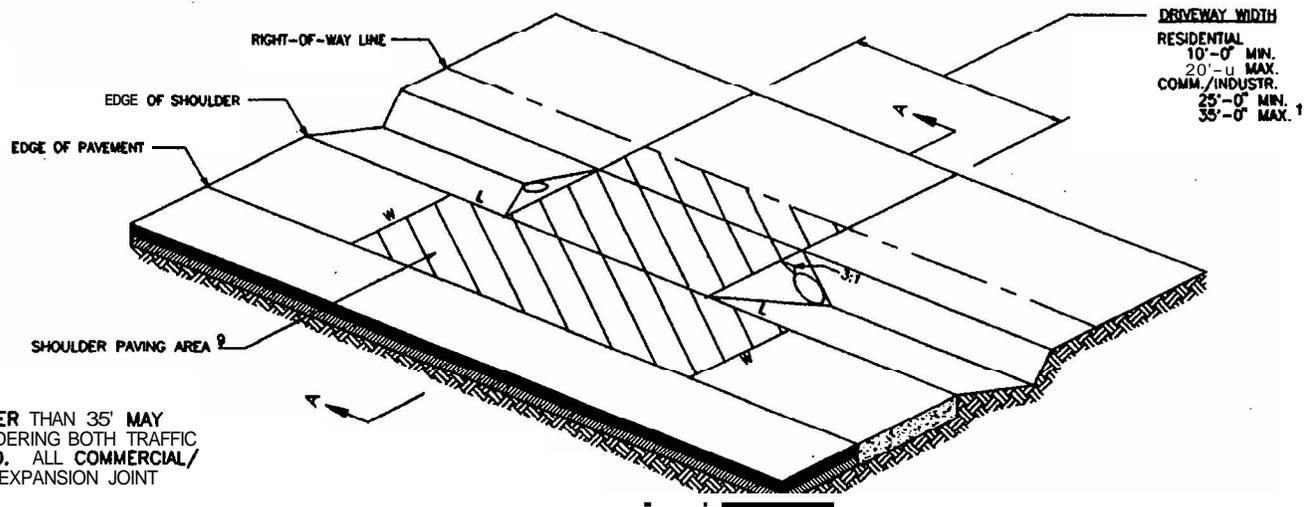
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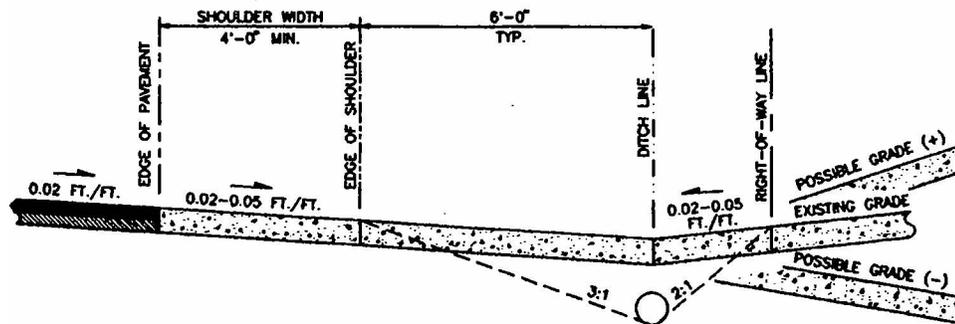
CURB DETAILS

DWG. 3-002
NO.



NOTES:

1. **COMMERCIAL/INDUSTRIAL** DRMWAYS WIDER THAN 35' MAY BE **APPROVED** BY THE ENGINEER CONSIDERING BOTH TRAFFIC SAFETY AND THE ACTMM BEING SERVED. ALL **COMMERCIAL/INDUSTRIAL** DRMWAYS SHALL HAM AN EXPANSION JOINT LOCATED MID-WIDTH. SEE SEC. 3.04.
2. PIPE SHALL BE:
A. SIZED TO **CONVEY** COMPUTED STORM WATER RUNOFF. AND
B. MIN. 12" DIAM., AND
C. **EQUAL TO OR LARGER** THAN EXISTING PIPES **WITHIN 500'** UPSTREAM.
3. EXPOSED PIPE ENDS SHALL BE B.M.L.E.D TO MATCH THE SLOPE FACE AND PROJECT NO MORE THAN 2" BEYOND SLOPE SURFACE. PROJECTING HEADWALLS ARE NOT ACCEPTABLE.
4. CONCRETE PIPE SHALL HAM MIN. COVER OF 6" TO FINISH GRADE. ALL OTHER TYPES OF PIPE SHALL HAM MIN. 12" COVER.
5. PIPE SHALL BE INSTALLED IN A **STRAIGHT** UNIFORM ALIGNMENT AT A MIN. 0.5% SLOPE (0.5 FT. PER 100 FT.) WITH THE DOWNSTREAM END LOWER THAN THE **UPSTREAM** END.
6. PIPE **MAY BE OMITTED** IF ROADSIDE DITCH DOES NOT **EXIST** AND **DRIVEWAY** DOES NOT BLOCK NATURAL FLOW.
7. **DRIVEWAY** SLOPE SHALL **MATCH** TO BACK EDGE OF SHOULDER. BUT SHOULDER SLOPE AND EDGE OF SHOULDER SHALL NOT BE ALTERED AS A RESULT OF DRMWAY CONSTRUCTION.
8. PAVED DRMWAYS SHALL BE P.A.M.D THROUGH RIGHT-OF-WAY WITH AC. OR B.S.T. BUT NOT P.C.C.
9. **GRAVEL DRIVEWAYS** SHALL BE **PAVED** BETWEEN THE EDGE OF PAVEMENT AND **R/W** WITH AC. OR B.S.T. ONLY WITH DIMENSIONS L=W.
10. SEE SEC. 3.01 AND 4.01.

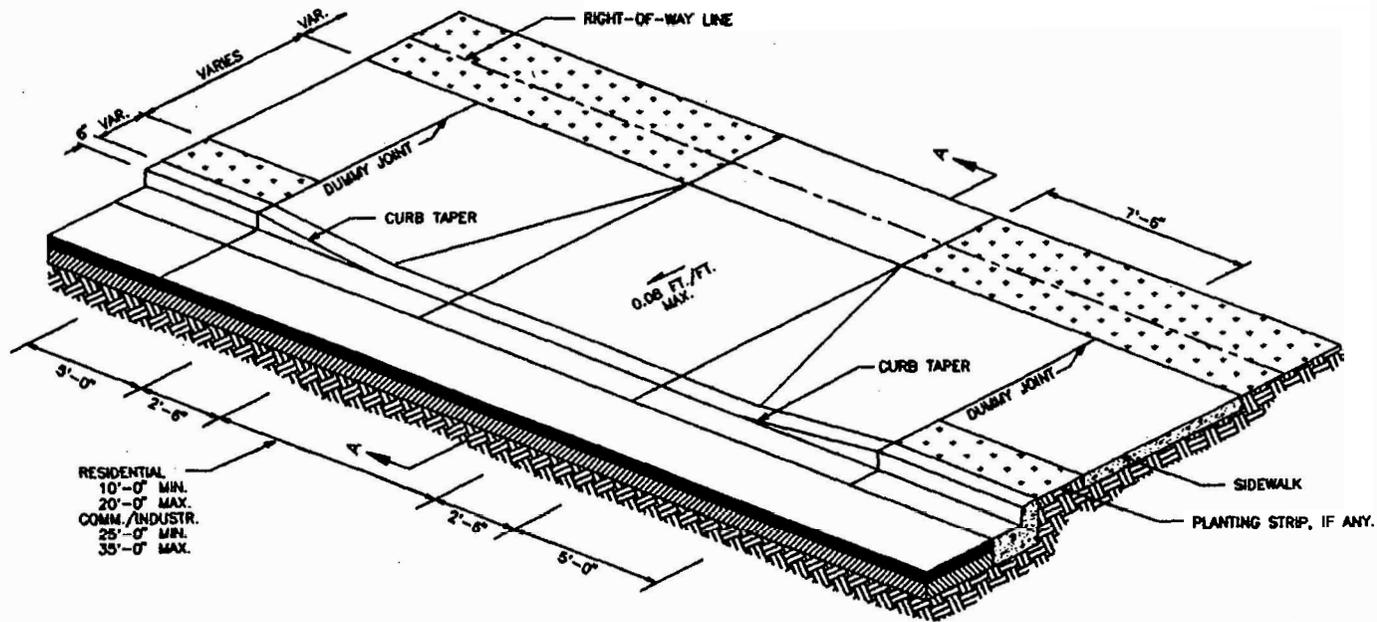


SECTION A-A

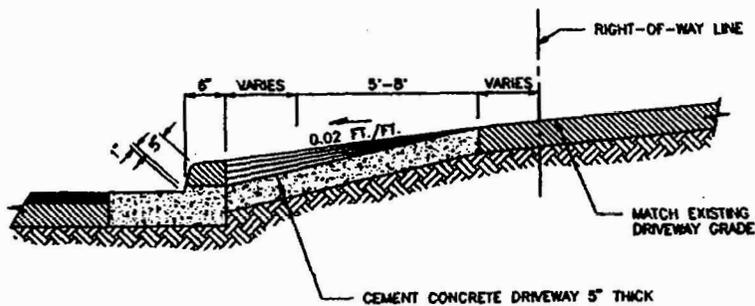
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SHOULDER & DITCH SECTION DRIVEWAY

DWG. NO. 3-003



RESIDENTIAL
10'-0" MIN.
20'-0" MAX.
COMM./INDUSTR.
25'-0" MIN.
35'-0" MAX.



SECTION A-A

NOTES:

1. COMMERCIAL/INDUSTRIAL DRIVEWAYS WIDER THAN 35' MAY BE APPROVED BY THE ENGINEER CONSIDERING TRAFFIC SAFETY AND NEEDS OF THE ACTIVITY SERVED. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH. SEE SEC. 3.04.
2. SEE SEC. 3.01.
3. SEE SEC. 4.01 FOR SURFACING REQUIREMENTS.

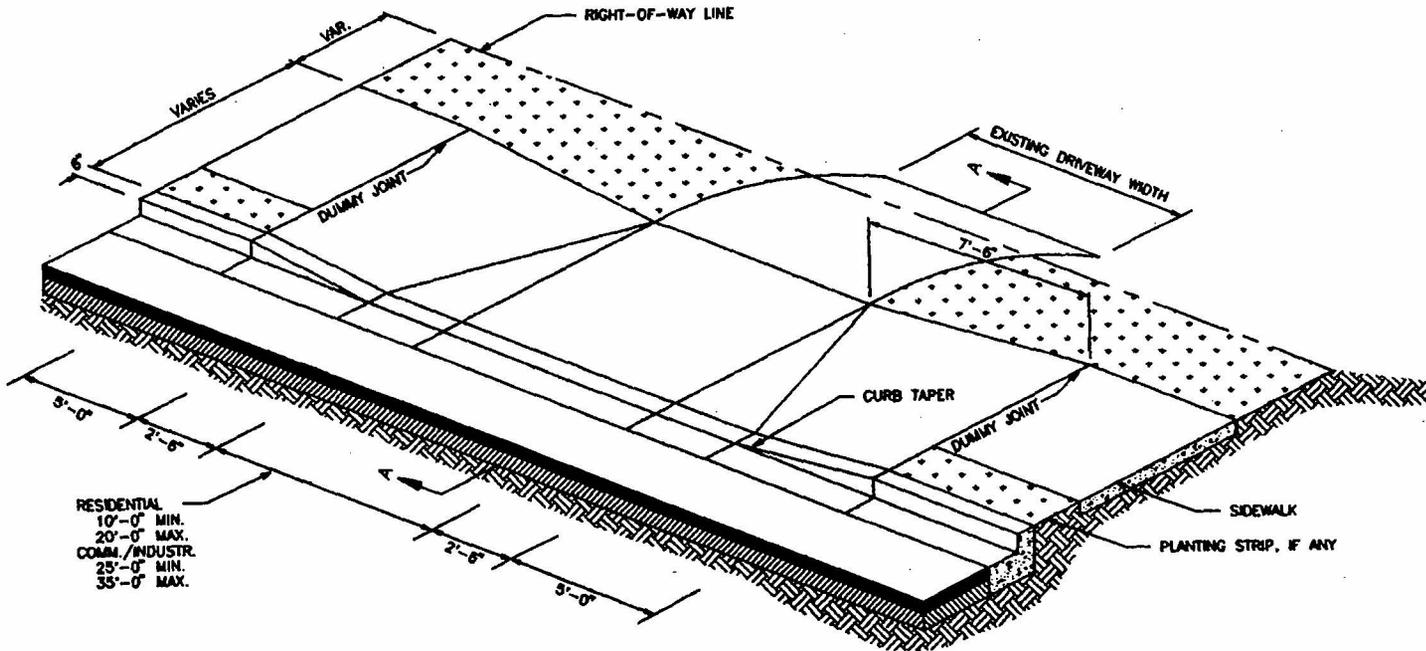
DATE	REVISION	BY	APPR'D



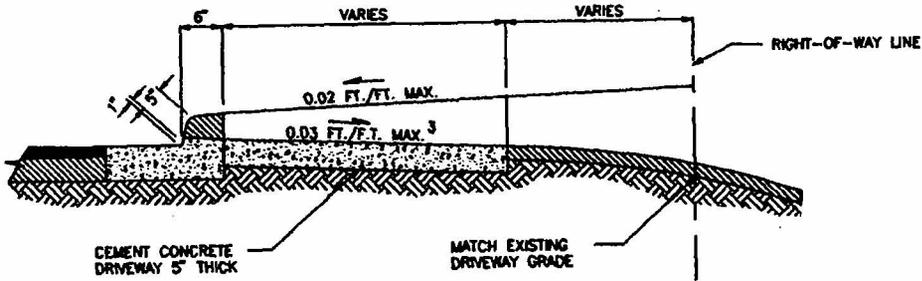
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CURB & GUTTER SECTION DRIVEWAY

DWG. 3-004
NO.



RESIDENTIAL
 10'-0" MIN.
 20'-0" MAX.
 COMM./INDUSTR.
 25'-0" MIN.
 35'-0" MAX.



SECTION A-A

NOTES:

1. A REVERSE SLOPE DRIVEWAY IS SUBJECT TO APPROVAL BY ENGINEER CONSIDERING NEED FOR AND COMPATIBILITY OF THIS FEATURE.
2. COMMERCIAL/INDUSTRIAL DRIVEWAYS WIDER THAN 35' MAY BE APPROVED CONSIDERING TRAFFIC SAFETY AND NEEDS OF THE ACTIVITY SERVED. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH. SEE SEC. 3.04.
3. A STORM SEWER INLET SHALL BE LOCATED WITHIN 20' BUT NO CLOSER THAN 10' UPGRADE FROM NEAREST EDGE OF CURB TAPER.
4. SEE SEC. 3.01.
5. SEE SEC. 4.01 FOR SURFACING REQUIREMENTS.

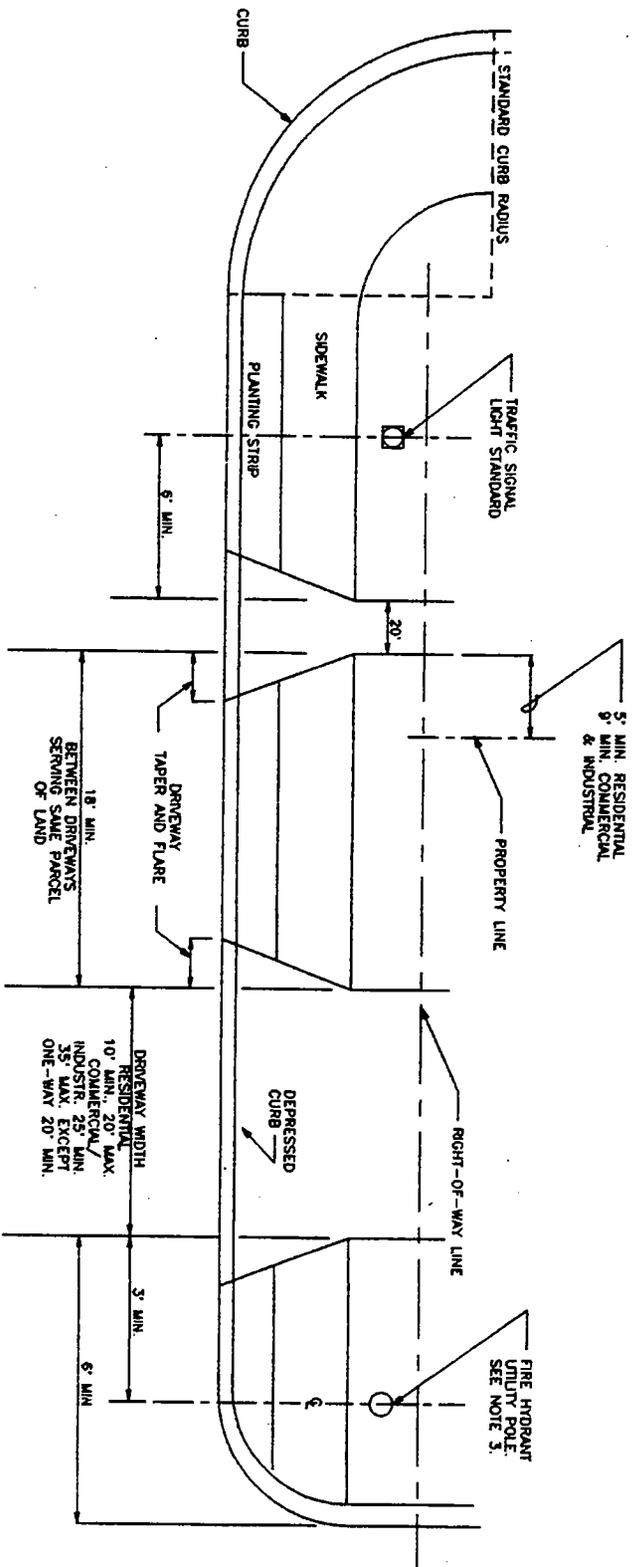
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REVERSE SLOPE DRIVEWAY

DWG. NO. 3-005



NOTES:

1. NO PORTION OF ANY DRIVEWAY SHALL ENCROACH IN CURB RETURN.
2. COMMERCIAL/INDUSTRIAL DRIVEWAYS MUST BE APPROVED BY THE ENGINEER, CONSIDERING BOTH TRAFFIC SAFETY AND THE ACTIVITY BEING SERVED. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH. SEE SEC. 3.04.
3. FOR ROADWAY CLEARANCE OF UTILITY POLES AND STRUCTURES SEE SEC. 8.02G AND DWG. NO. 5-001.
4. DRIVEWAYS SHALL BE LOCATED AS FAR FROM THE INTERSECTION AS POSSIBLE.

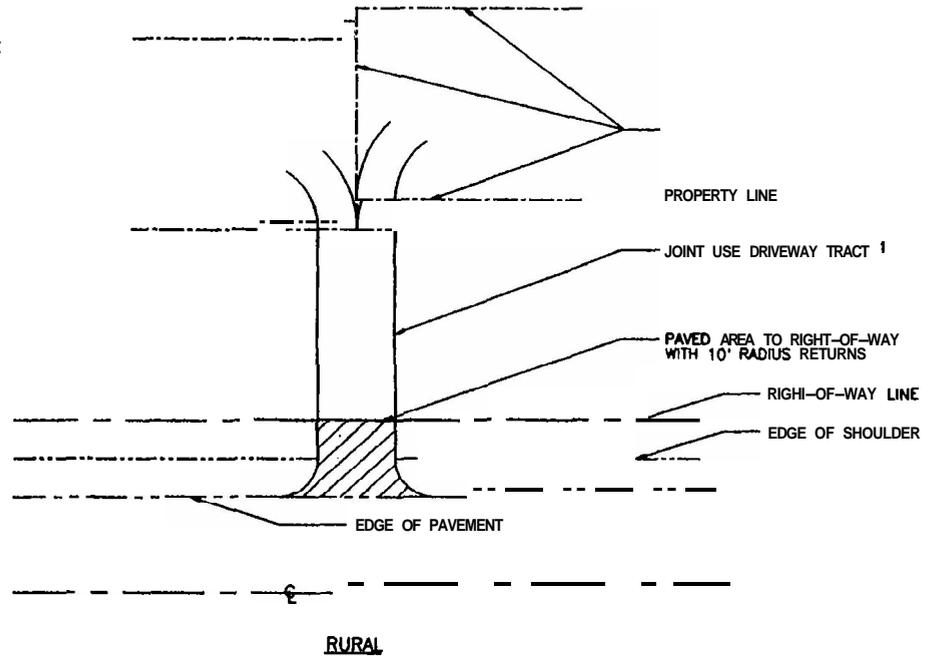
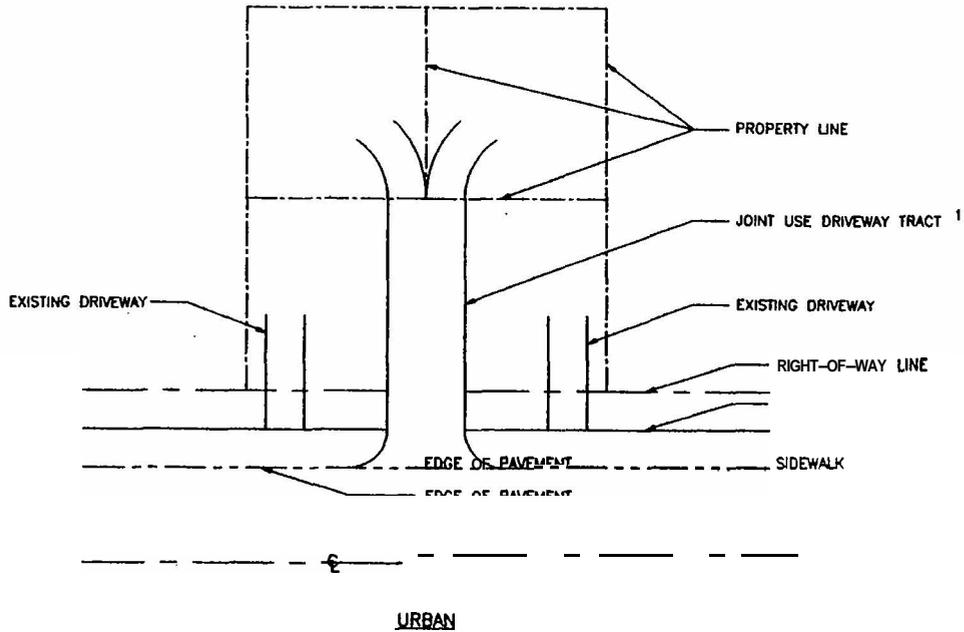
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LOCATION & WIDTH OF NEW DRIVEWAYS

DWG. NO. 3-006



NOTE:

1 SEE SEC. 3.01 FOR TRACT WIDTH AND PAVING REQUIREMENTS.

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JOINT USE DRIVEWAY TRACT

DWG. NO. 3-007